

# Roosevelt Main Street

## Revitalization Plan



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# INTRODUCTION

## Main Street

Main Street is part of Highway 40/191, which is the major highway in Northeastern Utah, connecting the Uintah Basin to Salt Lake City and the Wasatch Front. It is also part of the Federally designated Dinosaur Diamond Prehistoric Highway.

Main Street is an important transportation corridor in Roosevelt, accommodating approximately 10,000 vehicles per day (2016 UDOT stats), and it is owned and controlled by the Utah Department of Transportation. As a major energy hub in the heart of the Uintah Basin, Main Street is also relied upon to carry significant truck traffic, with truck traffic comprising 42% of all vehicles (2016 UDOT stats).

## Downtown Roosevelt

The intersection of Main Street and Lagoon Street is the center of the historic downtown area, and the historic buildings largely remain intact, forming a distinct and walkable downtown area.

However, over the years, time has taken a toll on many of the buildings. Most of the buildings, and the street itself, are at a point where a revitalization strategy is essential to ensure Main Street remains a functional commercial center that Roosevelt can be proud of.



Image 1- Main Street in the Early 20th Century

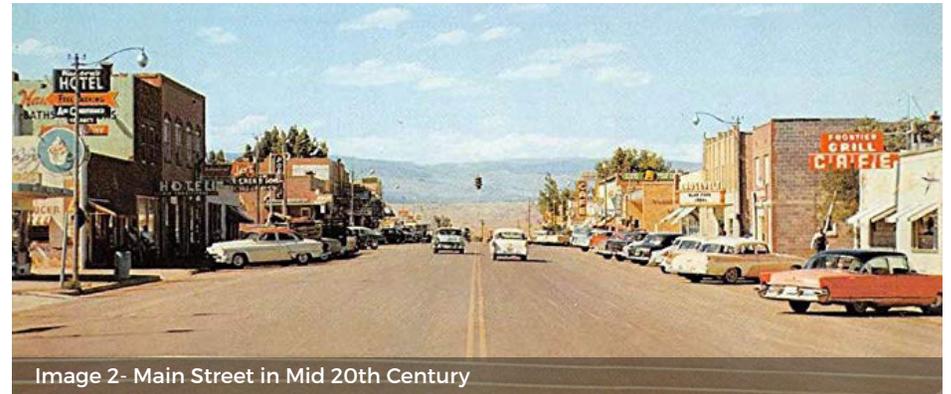


Image 2- Main Street in Mid 20th Century



Image 3- Main Street Today

# Study Area Boundary

Downtown Roosevelt is defined as the four blocks at the intersection of Main Street and Lagoon Street, as shown in Figure 1. The Downtown Transition areas that are shown on this map represent the areas to the north and south of the downtown core. These areas are outside of the study area boundary for this report, but some of the recommendations in this document may also be appropriate for these transition areas.

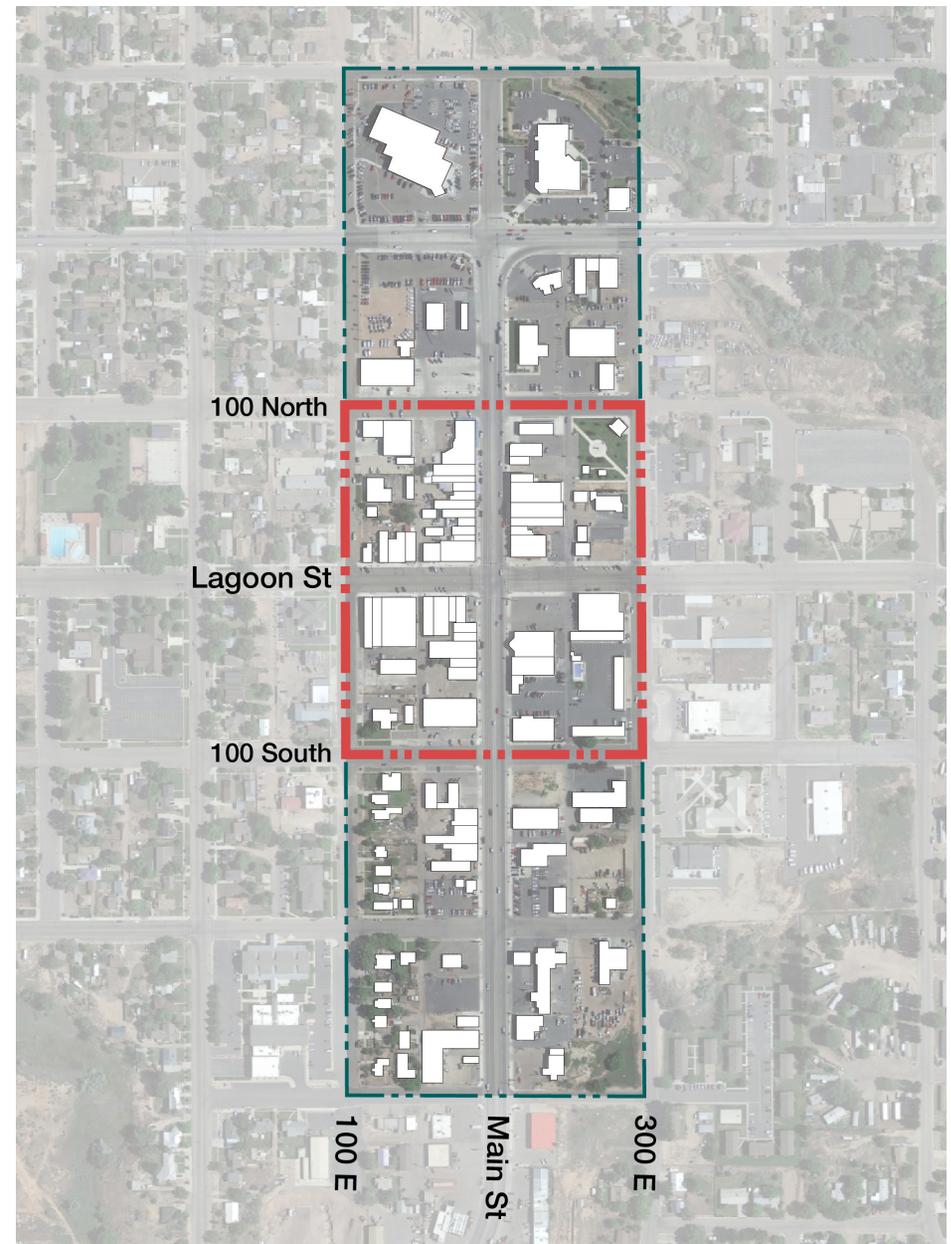
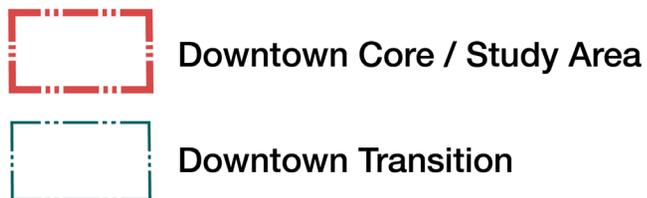


Figure 1 - Study Area

# Public Survey Summary

As part of the planning process, an online survey was conducted to gather feedback on how residents and visitors feel Main Street could be improved. More than 360 people participated in the survey. Of those, 84% live and/or work in Roosevelt, and 67% visit Main Street at least a few times a week. The following is a summary of the survey results:

## Existing Conditions

- Most people feel they “know” when they are in downtown and there is a sense of arrival, but 46% think it could be better.
- The majority of people feel safe walking along Main Street, though 37% think it could be improved.
- The vast majority of people feel the speed limit is appropriate, but there are issues with people speeding through.
- The majority of people feel crossing Main Street could be safer.
- There are some places that could use increased lighting.
- People are split if traffic is too noisy or not.
- The majority of people ranked their overall impression of Main Street in the 5-7 range, on a scale 1 – 10.
- When asked how well Main Street is currently serving the community, results quite evenly spread across the 3-7 range, on a scale 1 – 10.
- People mostly appreciate Main Street for its historic feel, and most people appreciate the variety of restaurants, shops, and services.

## *Additional Concerns (summary of comment section):*

- A significant amount of comments addressed the need for Main Street to be updated with a facelift. There is a general concern about the lack of aesthetic appeal and that it feels rundown.
- General cleanliness of the street/sidewalks/storefronts is a concern.
- Better parking is needed. Parallel parking on Main can be dangerous.
- Street trees would go a long way to creating a nicer experience.
- Safer street crossings needed.
- Need more shops/stores/restaurants/things to do and reasons to visit. Need to fill the empty storefronts.
- Traffic and trucks move through too fast and are too loud.

## Future Vision

- Most people would like to see more restaurants and shops (both local and/or chain), entertainment options, and community services added to/increased on Main Street. Not much desire for office and residential uses.
- Regarding Streetscape enhancements:
  - Top tier: Improved storefronts, street trees, benches and planters.
  - Middle tier: Outdoor dining and safer street crossings
  - Bottom tier: Decorative pavers, covered arcade, and wayfinding signage.
- The streets that Roosevelt can emulate can be summarized as: “Vernal would be nice, Heber City would be great, Steamboat Springs would be ideal”

## General Comments (summary of comment section):

- “Keep its historic feel but become more of a destination”
- Additional accent/mood lighting, like trees, bistro, etc.
- More parking off the street. Street parking can be dangerous.
- “Keep it simple and old fashioned.”
- “I would love to see Main Street as a destination. A place where people gather both in the day and at night. I’d love to see the old advertisements touched up and repainted on the brick buildings. I want to see more than empty storefronts, rundown buildings, dance studios and hair salons. Places where people actually want to be like live theater or music, little cafes or something like that”

*“It would be so wonderful to see Roosevelt make Main Street a unique and inviting place.”*

# VISION +

## PROJECT GOALS

*The vision for the Roosevelt Main Street Revitalization plan is to restore Main Street to prominence as Roosevelt's heart and commercial center, as well as to enhance its general attractiveness, walkability, and to make it more inviting.*

### **Project Goals**

1. Update Building Facades
2. Support Existing Businesses and Encourage New Businesses in Empty Storefronts
3. Enhance the Public Realm
4. Enhance Safety and Comfort Along Main Street
5. Establish a Unique Identity that is Authentic to Roosevelt
6. Improve Parking Safety and Quantity



# FRAMEWORK

# PLAN

Main Street and Lagoon Street are two of the central and most important streets in Roosevelt. Main Street forms the primary travel and commercial corridor in the City. Lagoon Street, on the other hand, contains many of the City's major civic amenities and attractions, including Constitution Park, the Aquatic Center and the Library to the West. To the East is the major education facilities in town, with Union High School, Uintah Basin Technical College, and Utah State University - Uintah Basin Campus.

## Lagoon Street

Lagoon Street is the central east-west street in Roosevelt, and a significant number of Roosevelt's major attractions are located along the street. Due to these factors, Lagoon Street should be considered a significant corridor that connects the numerous attractions. The street should be the primary east-west travel corridor, with a specific focus on providing safe and convenient travel for pedestrians, bikes, and other non-vehicular modes of travel.

## Cottonwood Creek Trail

Cottonwood Creek is an underutilized amenity in Roosevelt that has the potential to be an incredible asset for Roosevelt residents. A trail is being proposed to run along the Creek. Additionally, the land around the creek, in particular the land to the south of Lagoon Street, has the space and natural assets to become a park-like amenity.

This future trail and park system is easily accessed from Lagoon Street, and therefore, reinforces the need for strong pedestrian and bike connections from Downtown to the many attractions along Lagoon Street.



Image 4 - Lagoon Street

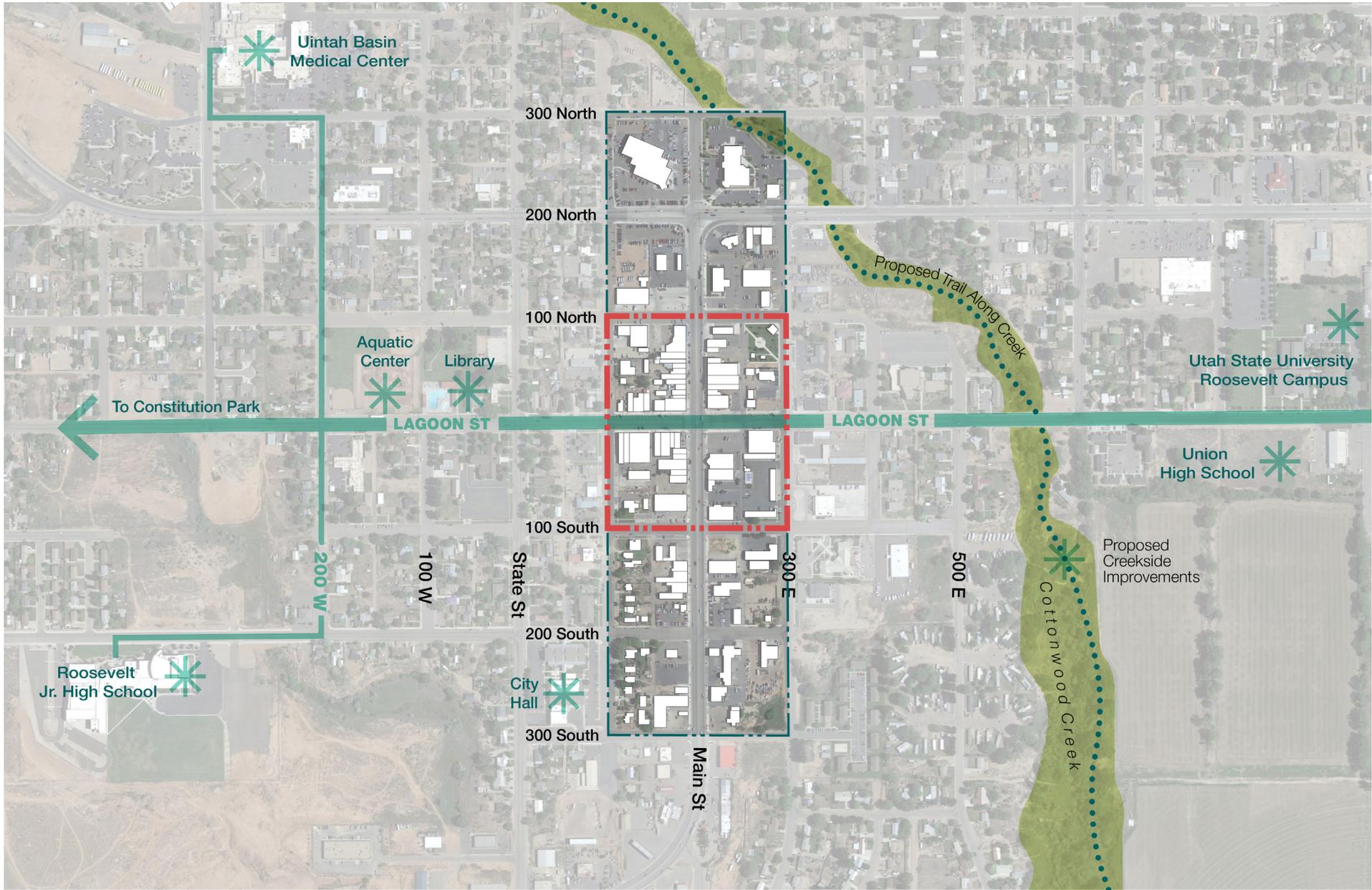


Figure 2 - Framework Plan

# STREETSCAPE DESIGN VISION

The streetscape refers to all public rights-of-way within the study area, including both the road and sidewalk area. Alleys should also be considered as part of the streetscape.

The streetscape is an essential element of a successful downtown, as a great downtown must, at minimum, provide a safe and pedestrian friendly environment.

A good streetscape will:

- Accommodate pedestrians in a safe and comfortable environment.
- Signal to vehicles passing through that this is a special area that requires them to slow down and pay extra attention for pedestrians.
- Provide pedestrian amenities such as lighting, seating, and more.
- There are many elements that contribute the quality of the streetscape, which are covered in this section. These include crosswalks, curb extensions, benches, planters, and more.
- Work in concert with the buildings and storefronts.

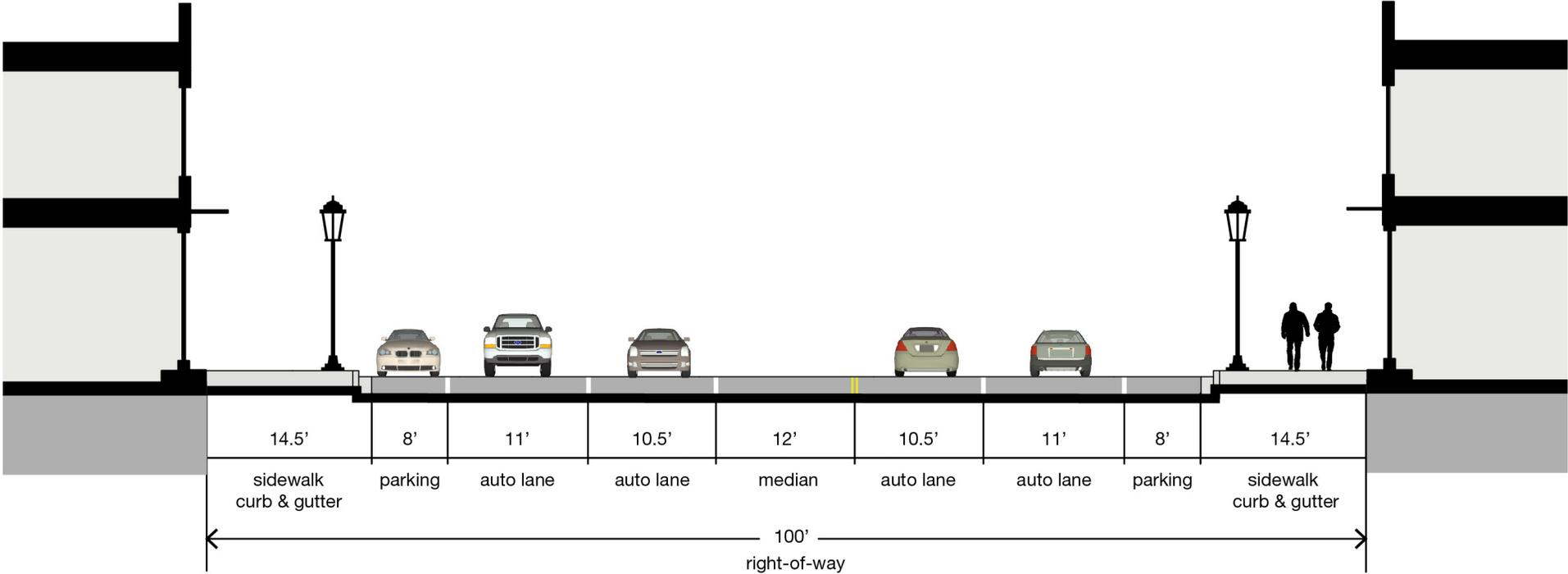


Image 5 - Highly Developed Streetscape in Richfield, UT



Image 6 - High Quality Building Facades in Provo, UT

**Figure 3 - Main Street Existing Street Section**



# Crosswalks

Crosswalks indicate to motorists where pedestrians have the right-of-way and where to yield. Crosswalks should be *highly visible* to both drivers and pedestrians.

Currently, only one crosswalk exists, which is at the intersection of Main Street and Lagoon Street. Crosswalks should also be created at additional intersections as shown in figure 4.

Crosswalks should be enhanced from their baseline design (as currently painted at the intersection of Main Street and Lagoon Street). There are a number of options to make crosswalks more visible, as shown in images 7 - 9. These include:

- Color Paint
- Thermoplastic
- Stamped Colored Asphalt (brick look)

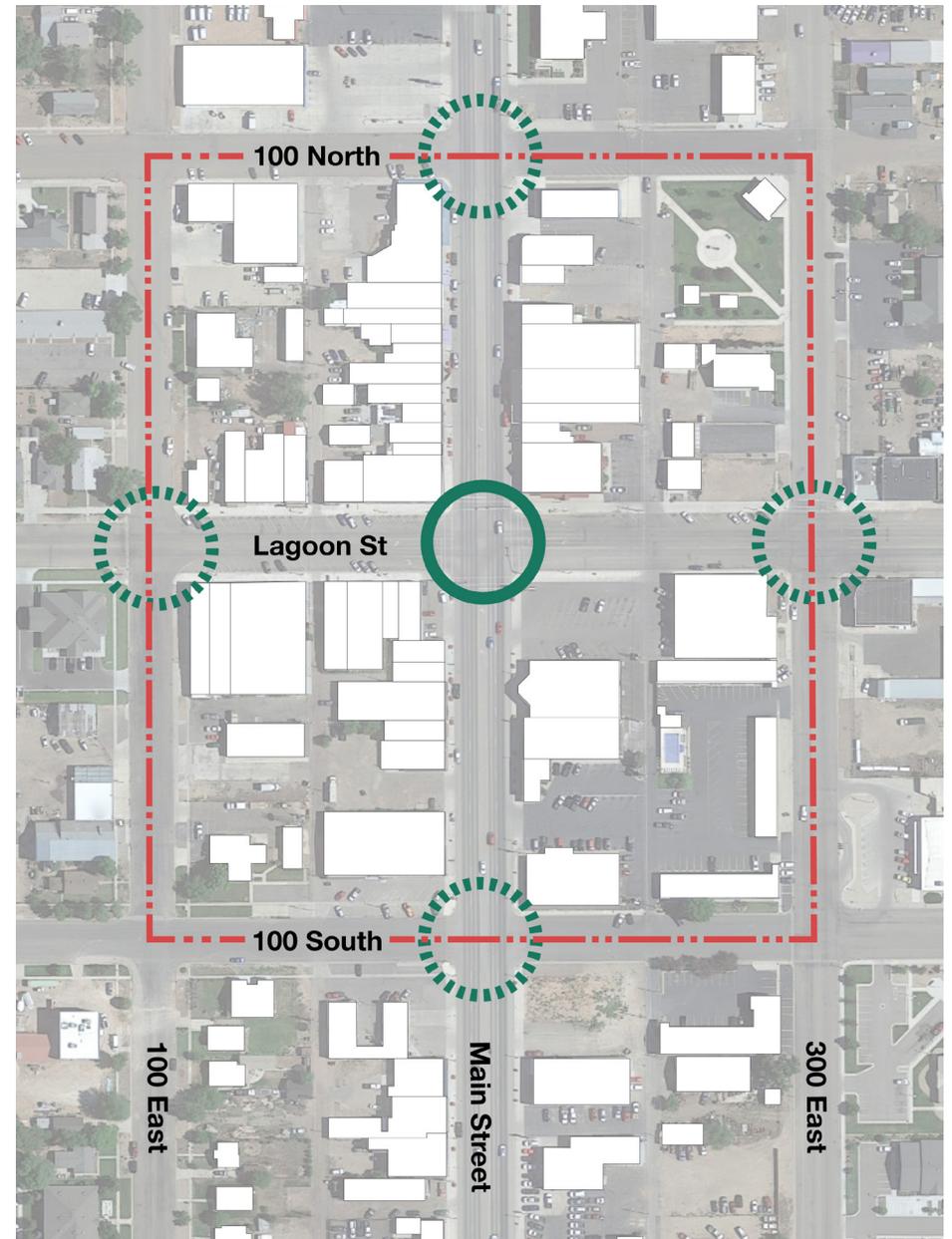
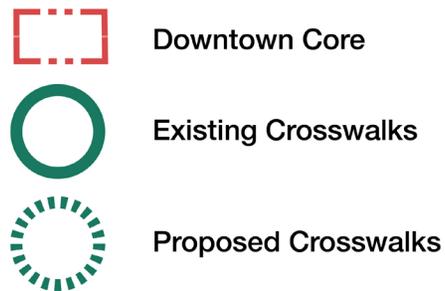
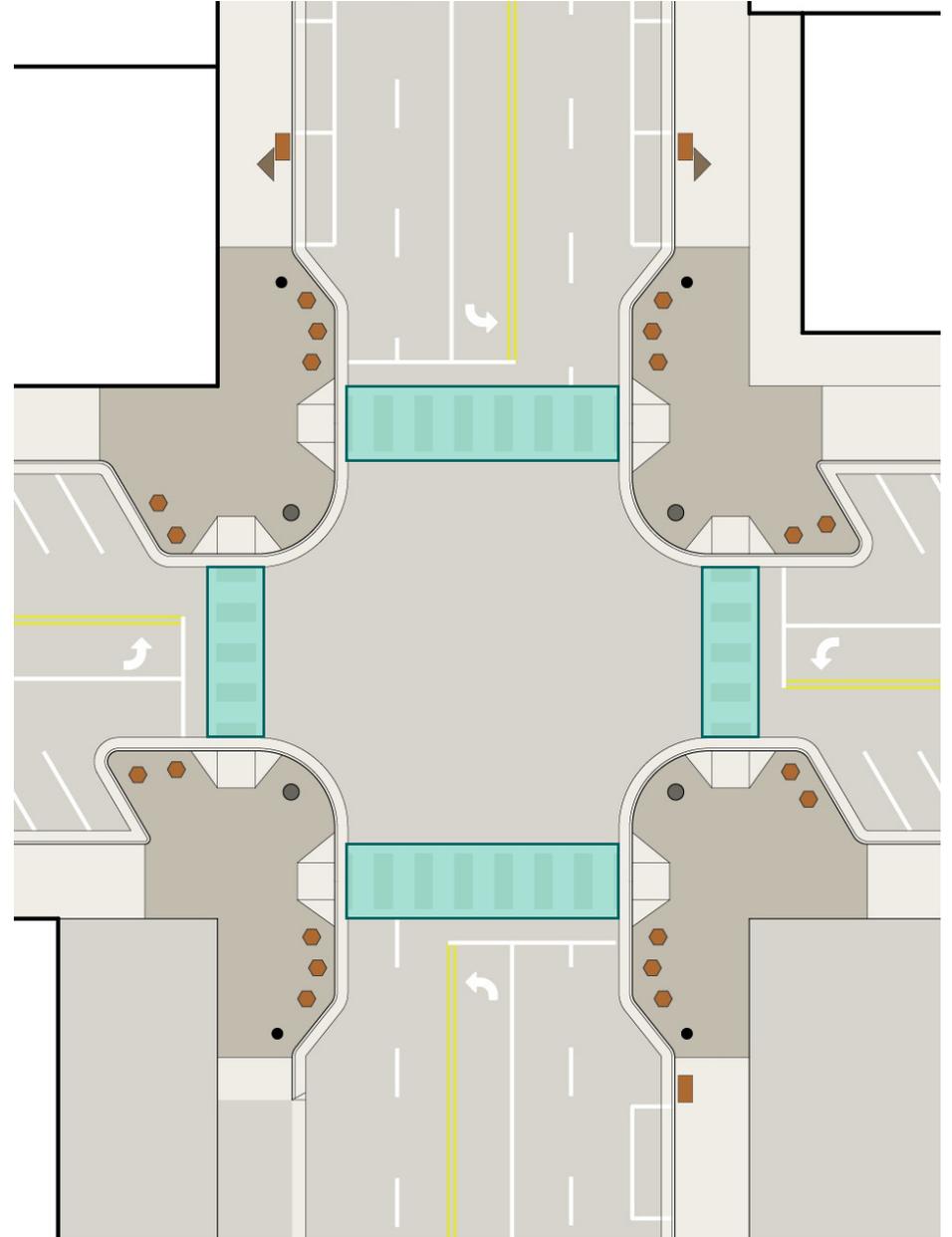


Figure 4 - Proposed Crosswalks



# Curb Extensions

A curb extension (also called bulb out) is an extension of the curb at an intersection that extends the sidewalk area into the parallel parking area of the roadway. See figure 7. Curb extensions are proposed on all intersections with the study area, as shown in Figure 6.

Curb Extensions provide a number of safety benefits for pedestrians, including:

- Increased pedestrian visibility at intersections through improved sight lines
- Decreased pedestrian exposure to vehicles by shortening the crossing distance
- Reduced vehicle speeds by physically and visually narrowing the roadway
- Increased pedestrian waiting space
- Additional space for street furnishings, plantings and other amenities
- Improving safety for parallel parking by clearly defining parallel parking area as separate from the travel lanes.



Downtown Core

Proposed Curb Extension

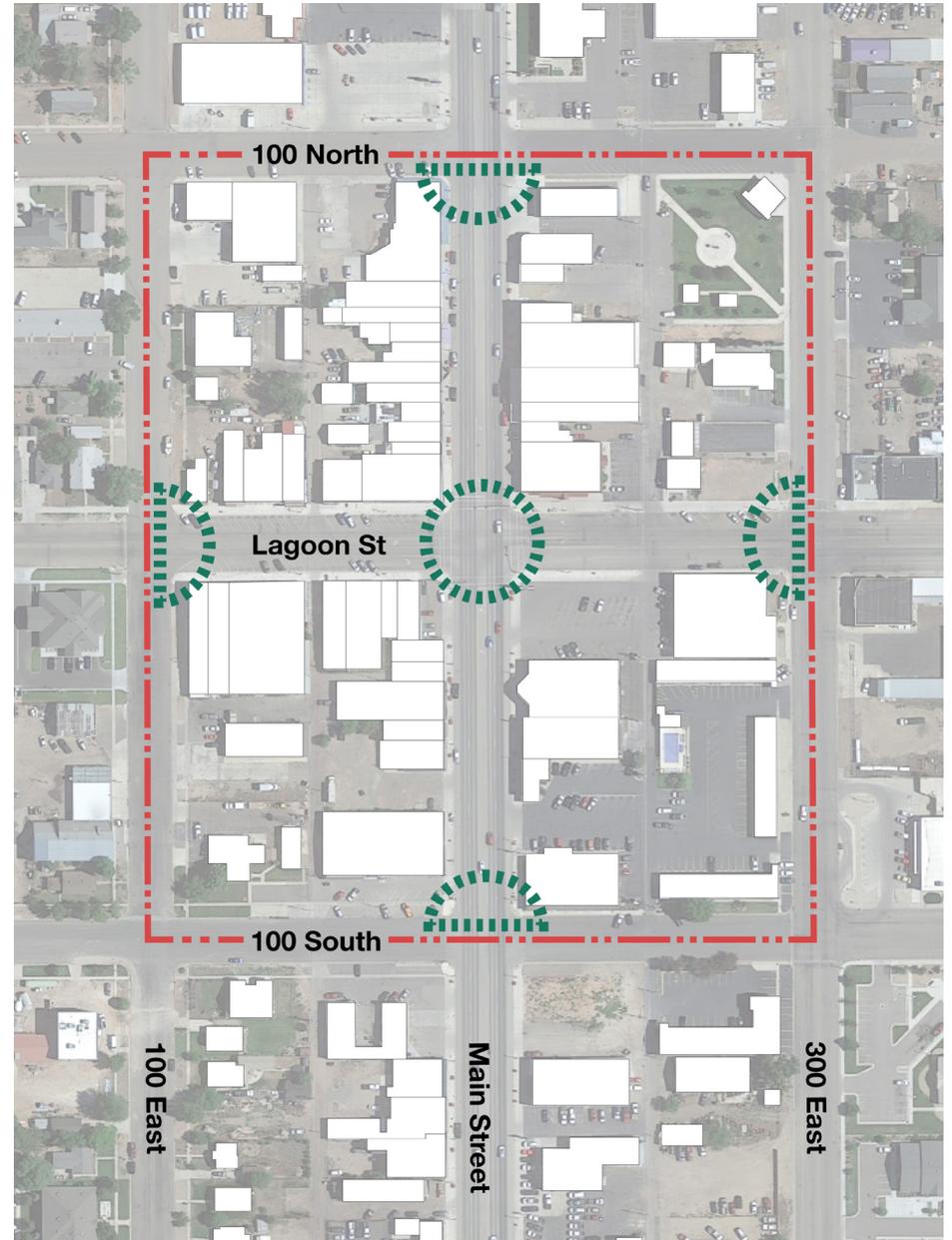


Figure 6 - Proposed Curb Extension



Image 10 - Curb Extension

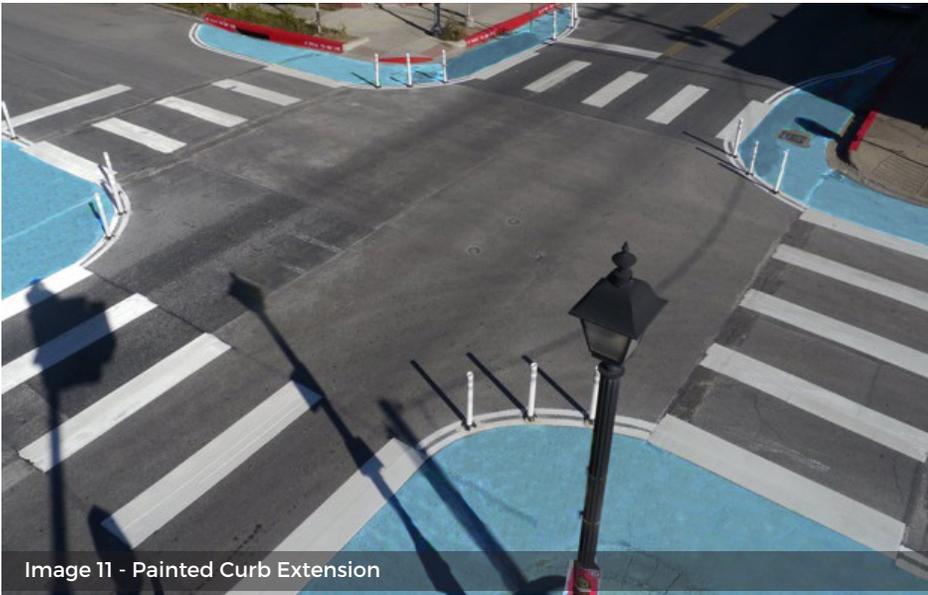


Image 11 - Painted Curb Extension

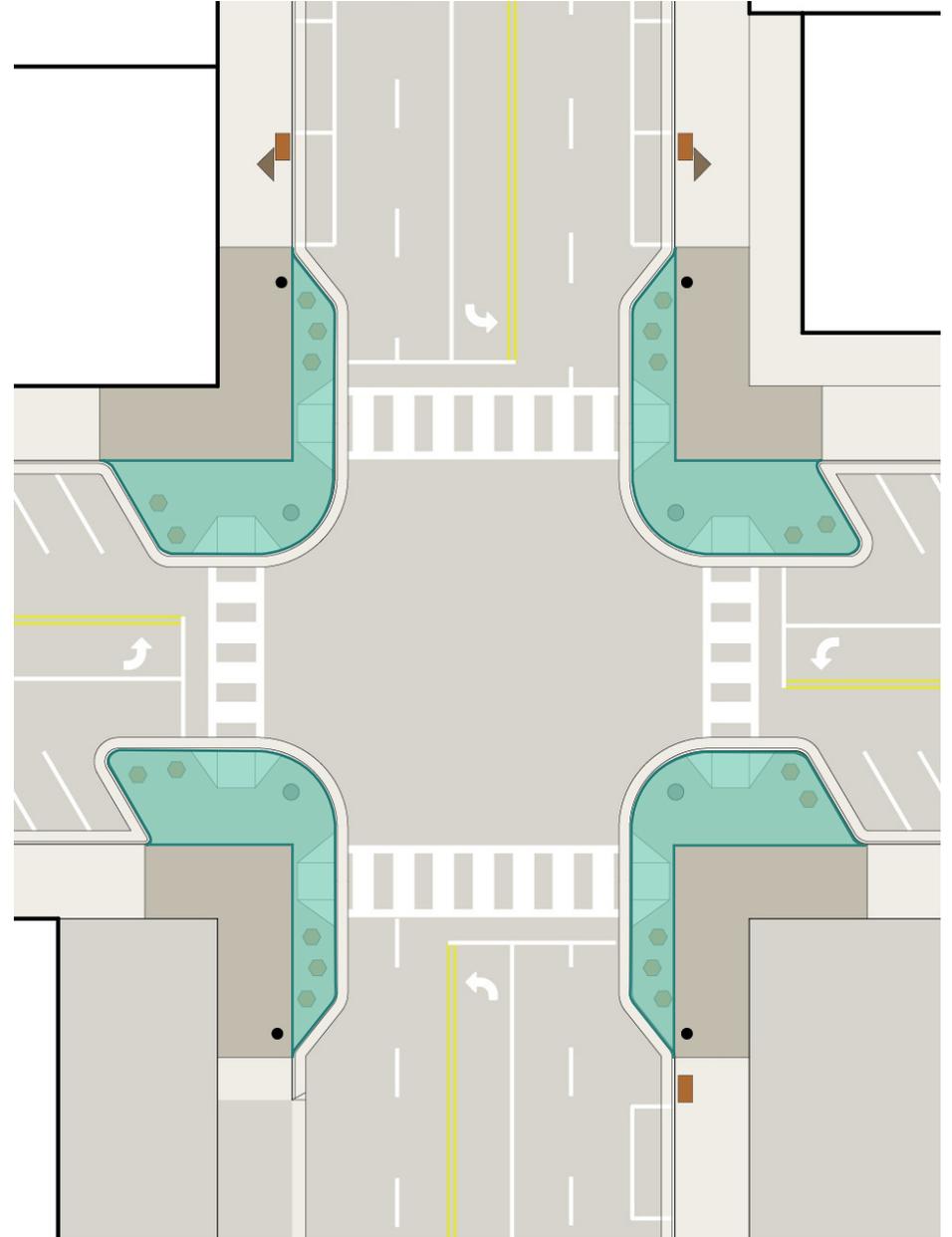


Figure 7 - Curb Extensions

# Streetscape Zones

The sidewalk can be broken into three distinct zones that organize the streetscape.

## Building Zone

The building zone is the space between the travel zone and the building facade. This zone can be used to display merchandise, enhance entryways, or provide outdoor seating and dining. It should generally be thought of as an extension of the building into the public realm.

## Travel Zone

The travel zone is reserved for unobstructed pedestrian travel. It is located between the building zone and the street zone. This space must remain at least five feet wide.

## Furnishing Zone

The furnishing zone is the space between the travel zone and the street. This area should be hardscape, and it is where street furniture, such as street lights, planters, benches, bike racks, garbage/recycling receptacles, and any other street furniture should be located. Tables and chairs for outdoor dining may also be placed in this zone.

# Streetscape Furniture

The street furniture and pedestrian amenities along Main Street should include:

- Lighting
- Trash/Recycling Receptacles
- Benches
- Planters
- Public Art

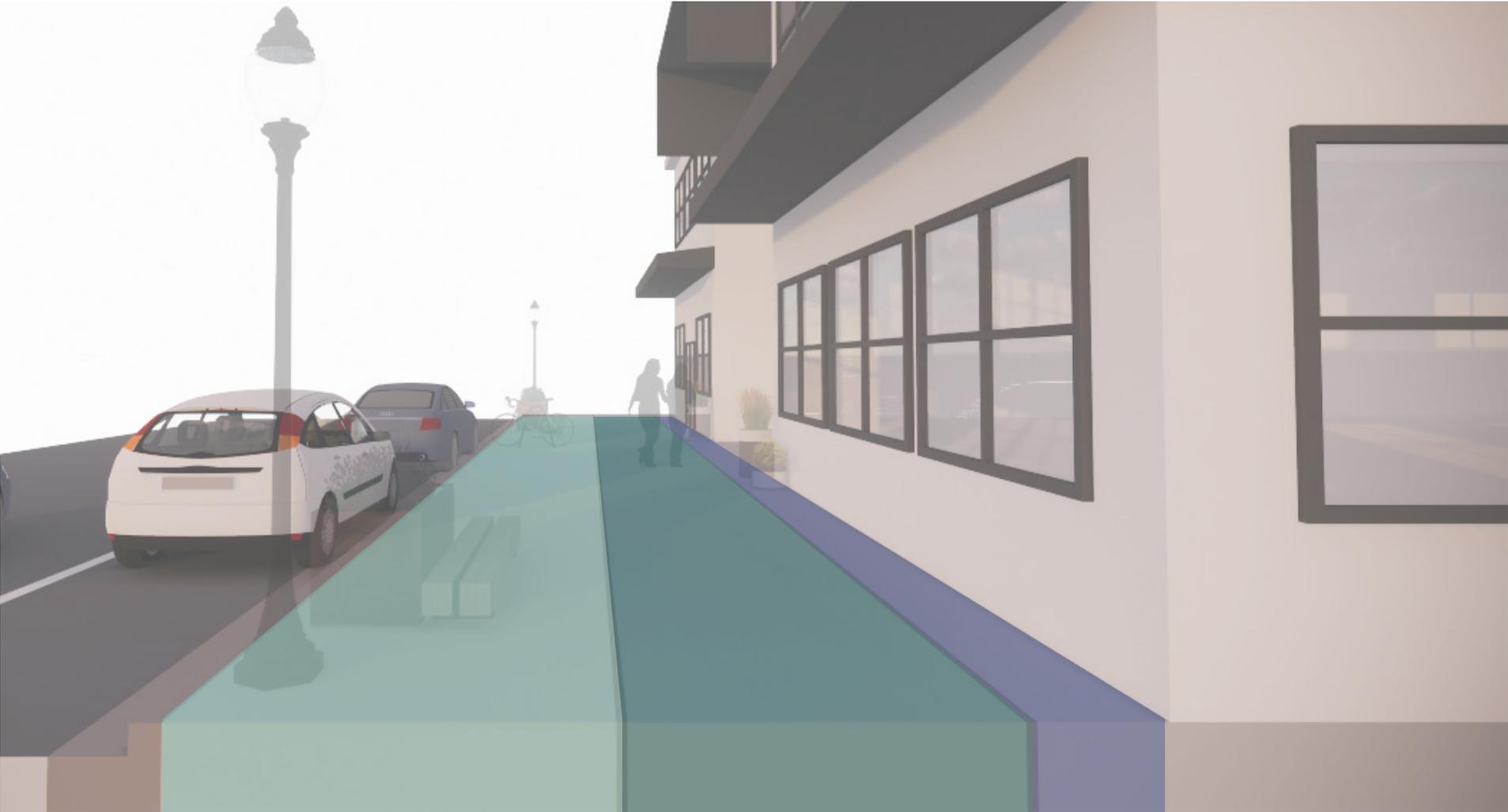
Other street furniture that may be added, if applicable, include:

- Bike Racks
- Tables and Chairs

## Furniture as Art

Streetscape furniture provides an opportunity to provide unique, artful elements along Main Street that can help Roosevelt stand apart from other cities. Specifically, planters and benches have been identified as street furniture with the ability to help create a distinct aesthetic for Roosevelt Main Street. See the Streetscape Planter and Bench sections on pages 20 - 23 for more.

**Figure 8 - Streetscape Zones**



**Furnishing  
Zone**

**Travel  
Zone**

**Building  
Zone**

# Streetscape Planters

Currently, Main Street has a number of planters that are proposed to be replaced as part of the revitalization project. The planters used along Main Street provide an exciting opportunity to create a unique aesthetic that is rooted in the culture of Roosevelt and will help define the image of the City.

Roosevelt and its residents have significant expertise in steel work and welding. As an energy hub, many of the city's residents are skilled welders, and additionally, the Uintah Basin Technical College has a world class welding program. The City should tap into this resource and expertise to create planters (or a variety of planters) that are unique to Roosevelt.

Images 12 - 14 show various examples of how steel has been used in other cities to provide a unique look that represents their respective towns.

Planters should be placed frequently along Main Street. This helps separate pedestrians from traffic, provides a sense of enclosure for pedestrians, and helps soften the environment with landscape. Ideally, these should be spaced every 50 feet. However, they should also be strategically placed around curb cuts and should also work with building entrances.

Planters should also be integrated into curb extensions.



Image 12 - Old Mining Cart Planter

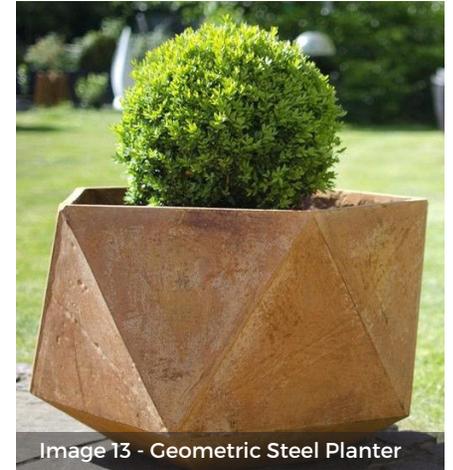


Image 13 - Geometric Steel Planter



Image 14 - Custom Steel Planter

## Planter Rendering

These images show a demonstration for how new steel planters can be integrated into the streetscape.

The top image shows general locations for planters, and the bottom image shows a demonstration of what the installation of unique steel planters might look like.

*The specific planters shown here are for demonstration purposes only. The final design/product should be determined at a later date.*

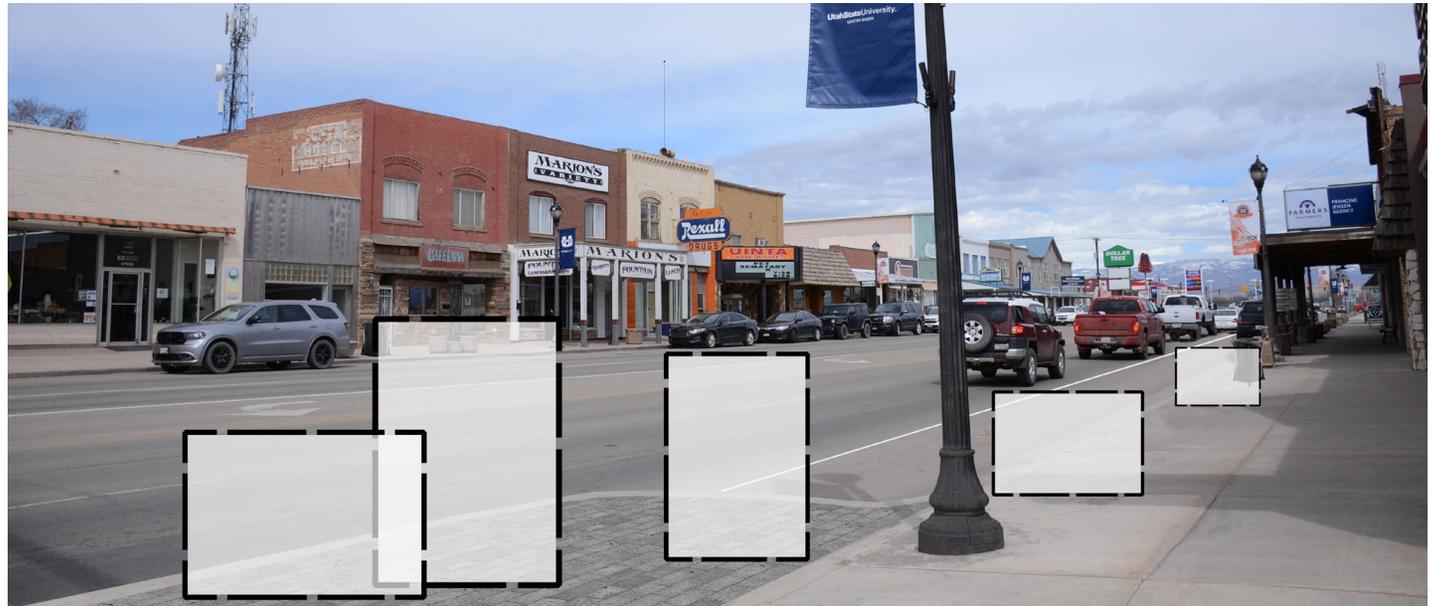


Figure 9 - Planter Locations and Rendering

# Benches

Currently, the city does not have any benches along Main Street. It is recommended that benches be installed, in conjunction with the new planters.

Similar to the planters, the benches are an opportunity to install a unique element along Main Street that could help Roosevelt Main Street stand out. Therefore, rather than traditional black metal benches, which are a staple of most small town Main Streets, it is recommended that the City install benches that are unique and complement the steel planters.

Images 15 - 18 show examples of unique benches found in other communities. Some of these are custom, while some are available to purchase from manufacturers. However, they are all non-traditional and would help distinguish Roosevelt Main Street in an interesting way.

Benches should be arranged along Main Street in conjunction with the planters. Ideally, each block face will have three to four benches that are evenly spaced along the street.



Image 15 - Custom Triangular Bench



Image 16 - Heavy Wood Bench



Image 17 - Unique Stone Bench



Image 18 - Gabion Bench

## Bench Rendering

These images show a demonstration for how a unique bench can be integrated into the streetscape.

The top image shows general locations for benches, and the bottom image shows a demonstration of what the installation of unique benches might look like.

*The specific benches shown here are for demonstration purposes only. The final design/product should be determined at a later date.*



Figure 10 - Bench Locations and Rendering

# Lighting

The existing decorative light poles along Main Street are in good condition and help distinguish Main Street as an important street. These lights should remain as street lights along Main Street within the study area.



Image 19 - Existing Light Pole

# Trash/Recycling Receptacles

New trash/recycling receptacles should be simple, functional, classic and cost efficient. They should be made of black metal, in order to complement the existing light poles. Images 20 - 21 demonstrate examples of traditional black metal trash receptacles.

Each block face should contain one to two trash/recycling receptacles.



Image 20 - Traditional Trash Receptacle

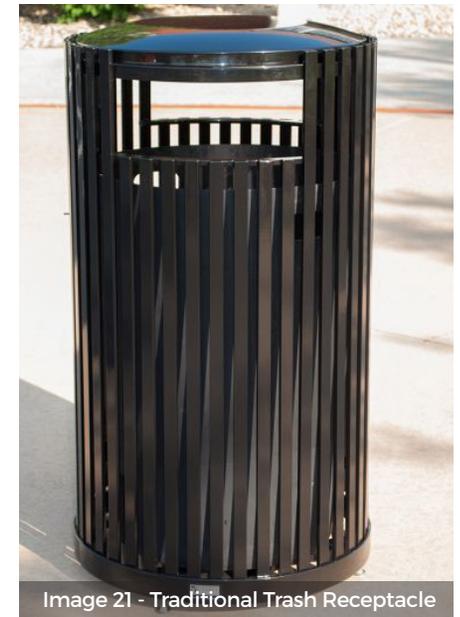


Image 21 - Traditional Trash Receptacle

# Outdoor Dining

Outdoor dining spaces are welcome along Main Street. Restaurants and/or other food services may utilize either the Building Zone or Furnishing Zone to accommodate tables and chairs.

The width of these zones are limited, which limits the possibilities for outdoor dining. Additionally, the large volumes of traffic and related noise may make outdoor dining along Main Street uncomfortable. To mitigate this, outdoor dining opportunities should be prioritized in off-street plazas, or side streets such as Lagoon Street.



Image 22 - Tables and Chairs for Outdoor Dining in Furnishing Zone



Image 23 - Tables and Chairs for Outdoor Dining in Building Zone

# Public Parking

The survey indicated that people feel unsafe parallel parking on Main Street, due to the fact that fast moving traffic and large trucks are so close to the parallel parking lane. The survey also indicated that more parking areas would be appreciated.

## Parallel Parking on Main Street

In order to address the safety concerns with parallel parking on Main Street, efforts should be made to enforce the speed limit, as well as clearly distinguish the parallel parking lane from the travel lane.

Curb extensions, as discussed on page 16-17, will help provide protection for the parallel parking lane, as well as further distinguish it from the travel lane. Curb extensions are also effective at slowing vehicles down. Additionally, parallel parking spaces should be clearly painted on Main Street, which will help to keep passing vehicles from drifting toward the parking lane.

## Additional Parking

Currently, there are angled on-street parking spaces striped on portions of Lagoon Street and on 100 North near Old Mill Park.

Additional parking spaces could be added/formalized by striping spaces on the remaining unstriped areas of Lagoon St and 100 North, as well as on the surrounding streets of 100 South, 100 East, and 300 East.

There are currently no vacant lots within close proximity to Downtown that could be converted to a parking lot. If the need for a dedicated parking lot arises, it should not have frontage on Main Street.

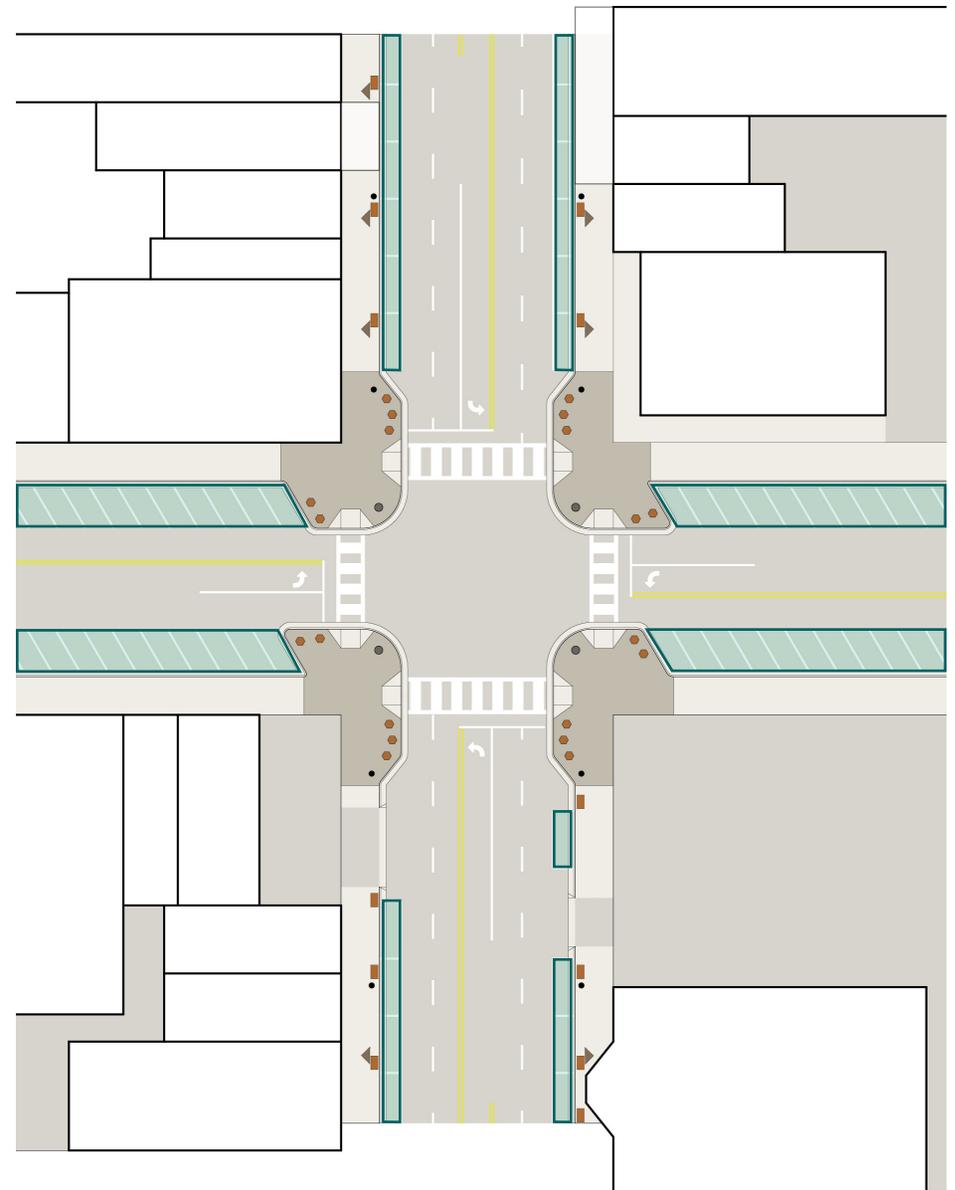


Figure 11 - On-Street Parking

# Alleys

Alleys are an important asset for the downtown area. They provide service and parking access for the buildings along Main Street, and allow the Main Street streetscape to be clear of large garbage bins and other obtrusive/unsightly elements. Alleys also accommodate utilities, such as power lines, allowing Main Street to remain uncluttered. Many of the buildings along Main Street also have secondary entrances that are accessed from the alley.

## Enhancing Alleys for Pedestrians

Alleys often get a bad reputation for being dirty and inviting crime. However, alleys have the potential to become an active and useful part of the downtown pedestrian experience, with some relatively simple improvements. This can be done by:

- Ensuring that alleys are cleaned up and organized so that they are safe for pedestrians.
- Cleaning up the rear facade of buildings.
- Adding murals and other art from local artists.
- Add lighting to increase safety at night. Adding unique overhead lights, as demonstrated in image 24, can also add to the interest and excitement of the place.
- Secondary Entrances can be added or enhanced on the rear facade of buildings.



Image 24 - Alley Activated with Lighting, Art, and Entrances



Image 25 - Alley Activated with Quality Paving and Secondary Storefronts

## BEFORE

Main Street, looking North.



Figure 12 - Existing Streetscape

## AFTER

This image shows a demonstration for how the Streetscape could be developed based on the criteria outlined on page 12 - 27.

- ① Trash Receptacle
- ② Unique Benches
- ③ Unique Steel Planters
- ④ Curb Extension



Figure 13 - Rendering Illustrating Concept of Streetscape with Unique Planter and Bench

# PUBLIC SPACE

## Existing Public Gathering Space

Currently, Old Mill Park is the only public gathering place in the downtown area. This half acre park functions as the town square, and contains a bandstand, pavilions, and picnic tables. This flexible park space has the ability to host a variety of community events, such as farmer's markets, festivals, concerts, and more.

## Potential Future Public Space Opportunities

The downtown area has the potential to add one or more small plazas that would ideally be directly adjacent to Main Street. While Old Mill Park provides a flexible open area that is ideal for large gatherings, any new open space that is created along Main Street should be designed as a small plaza to meet the specific needs of the users of Main Street. This plaza also has the potential to become an iconic and memorable place that will help define the image of Roosevelt. The following elements should be considered when developing new public open spaces.

- It should become a place of respite from the busy Main Street, with seating, trees, and other vegetation to mitigate the noise and traffic impacts.
- Major art pieces should be integrated into the plaza design that make the plaza memorable and help define the image of Roosevelt.
- Additional details such as unique lighting and paving can help create a great plaza.



Image 26 - Old Mill Park



Image 27 - Small Plaza with Memorable Public Art



Image 28 - Vacant Lot Converted to Outdoor Dining Area

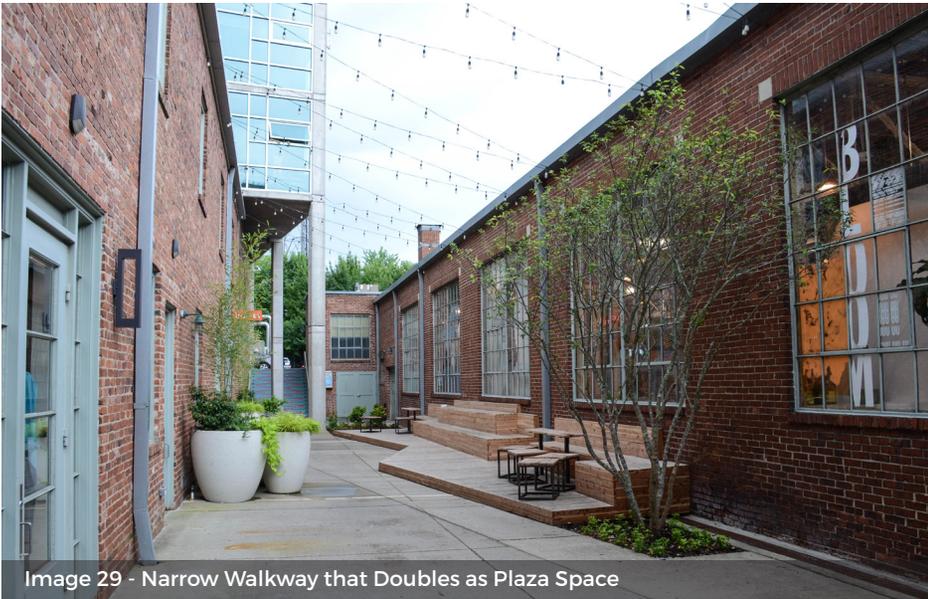


Image 29 - Narrow Walkway that Doubles as Plaza Space



Image 30 - Small Plaza with Coffee Vendor

## BEFORE

Existing Vacant Lot on Main Street



Figure 14 - Existing Vacant Lot

## AFTER

This image shows a demonstration for how a plaza could be developed based on the criteria outlined on pages 30 - 31.

- 1 Unique Lighting
- 2 Trees
- 3 Mural
- 4 Iconic Art Piece
- 5 Seating



Figure 15 - Rendering Illustrating Concept of Plaza with Iconic Art and Placemaking Features

# FACADE DESIGN

## GUIDELINES

The majority of buildings along Main Street (within the study boundary) are historic buildings with storefronts. This section contains design guidelines on how to revitalize these buildings in a manner that will greatly improve the building's aesthetic appeal and enhance the user experience for pedestrians.

Generally, historic storefront buildings share many common traits which help create a cohesive aesthetic and pedestrian friendly environment. These traits include building height and width, setback, proportions of door and window openings, and roof shape/profile. These traits are present on Roosevelt Main Street, and should be maintained with any renovations or new construction.

The intent of these guidelines is not to restore these buildings to their original historic condition. They are, however, intended to update and revitalize these buildings in a way that meets modern needs, while respecting the important benefits and principles of the historic architecture and design.

Any new buildings within the study area should be constructed to meet these guidelines as well.

### Building Elements Glossary

Figure 16 shows a typical commercial storefront building and calls out the various elements that are common to commercial storefront buildings.

**Parapet:** Low, protective wall at the edge of a roof.

**Cornice:** Horizontal Projecting brow at the top of the building or above the storefront.

**Pilaster:** Column projecting from the facade that defines the edges of the storefront.

**Signboard:** Space above the storefront that covers the structural beam, which can be used to attach signage. Awnings may also be attached on the signboard.

**Transom:** One small window or group of small windows, which can often be positioned above a door or larger storefront windows.

**Recessed Entry:** Helps to extend the display area and draw pedestrians inward.

**Display Windows:** Large windows at the street level of the facade to display merchandise and offer views in and out of the building.

**Bulkhead:** The lower panels on which the ground floor windows rest.

**Figure 16 - Typical Commercial Storefront Building and Elements**



# Facade Materials

The materials used on the facade of a building will have a significant effect on the visual impact and character of the building. The materials used should harken back to the historic materials that are original to the building and/or the City of Roosevelt. However, it is also understood that materials must be readily available and cost effective, in order to be implemented.

Therefore, materials should be chosen that have a classic and timeless look that will blend in with the historic aesthetic. Image 39 shows a successful example of newly restored facades with new, modern materials, juxtaposed with a historic brick storefront.

- Materials used should clearly differentiate between different storefronts and each building should have its own unique facade. Image 39 also demonstrates this principle.
- Secondary Materials should comprise no more than 30% of the non-glass facade materials.

## Facade Material Colors

The main colors used on building facades should utilize historic palettes from the region and/or major paint manufacturers.

Other colors may be strategically utilized to enhance details, accents, or otherwise provide interest and excitement to the facade and overall streetscape. These colors should comprise no more than 25% of the building facade. Image 34 shows a good example of utilizing non-traditional colors.

## Acceptable Primary Materials

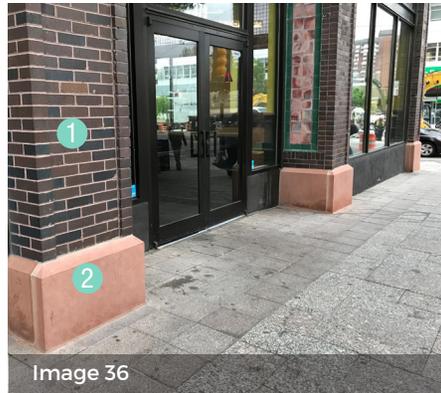
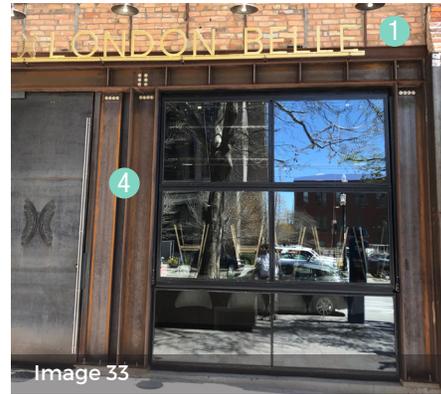
- 1 Brick
- 2 Natural Stone
- 3 Painted Wood
- 4 Steel
- 5 Cement Fiber Board

## Acceptable Secondary Materials

- 6 Stucco  
Stone Veneer

## Not Acceptable

- Metal Paneling
- False Roofs
- Ceramic Tile
- Polished Stone
- Rustic Wood



# Awnings

Awnings and canopies are commonly used on historic storefront buildings, and a number of buildings in the study area currently have awnings. Awnings provide shelter for shoppers and assist with climate control and solar shading.

- Awnings should be canvas on a sloped pitch, as demonstrated in image 42 or a horizontal steel awning, as shown in image 43



# Gallery

A Gallery is a type of covered entryway where an attached canopy covers the entire width of the sidewalk and is supported by columns. See image 44. There are a number of existing gallery storefronts within the study area.

- Galleries should be coordinated with neighboring buildings.
- Materials used on the galleries should match the building facade.
- These canopies may also be used as balconies and accessed from the second story, as demonstrated in image 45.



# Building Lighting

Exterior lighting is an important element of the building facade, as it illuminates the storefront, building signage, and primary entrances. It also helps enable a comfortable pedestrian experience at night.

- Light fixtures should be appropriate to the building's architectural style.
- Lights should cast downward, illuminating the storefront and the sidewalk.
- Lights should illuminate the primary entrance.



Image 46 - Goose Light Fixtures Illuminate Facade and Signage



Image 47 - Light Fixtures Illuminate Facade

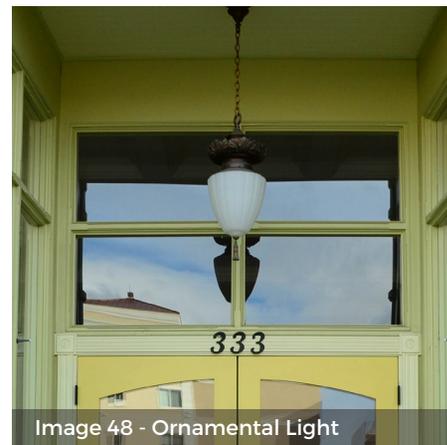


Image 48 - Ornamental Light



Image 49 - Lights Illuminate Entrance

# Ornamentation + Details

Architectural details such as cornices, parapets, pilasters, bulkheads, and many others are important elements of traditional storefront buildings. When revitalizing buildings on Main Street, these important details should be preserved, restored, or recreated, where possible. These types of architectural details have significant impact on creating an interesting and attractive storefront, as well as greatly enhancing the appeal of the streetscape. See Building Elements Glossary on page 34 for more detail on each of these architectural elements.

## Architectural Elements

- 1 Cornice
- 2 Parapet
- 3 Pilaster
- 4 Bulkhead
- 5 Decorative Masonry
- 6 Transom



Image 50 - Multiple Facades with Various Ornmentation and Details



Image 51 - Ornamental Bulkhead with Detailed Tile Entryway

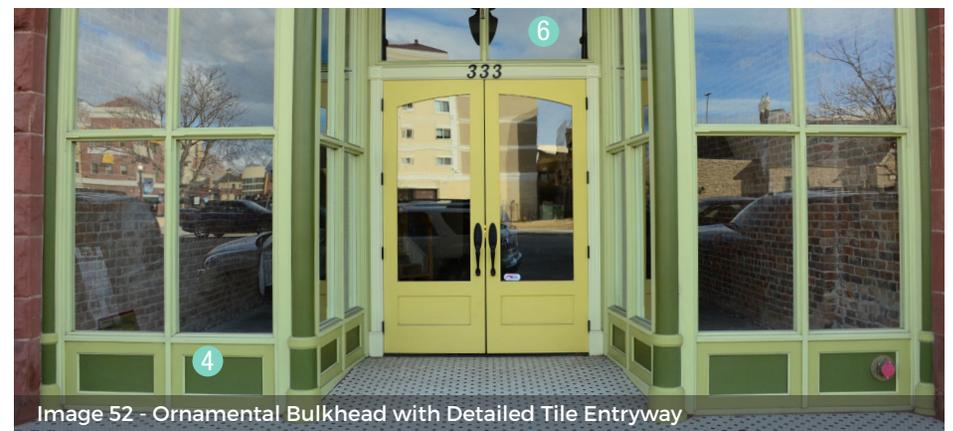


Image 52 - Ornamental Bulkhead with Detailed Tile Entryway

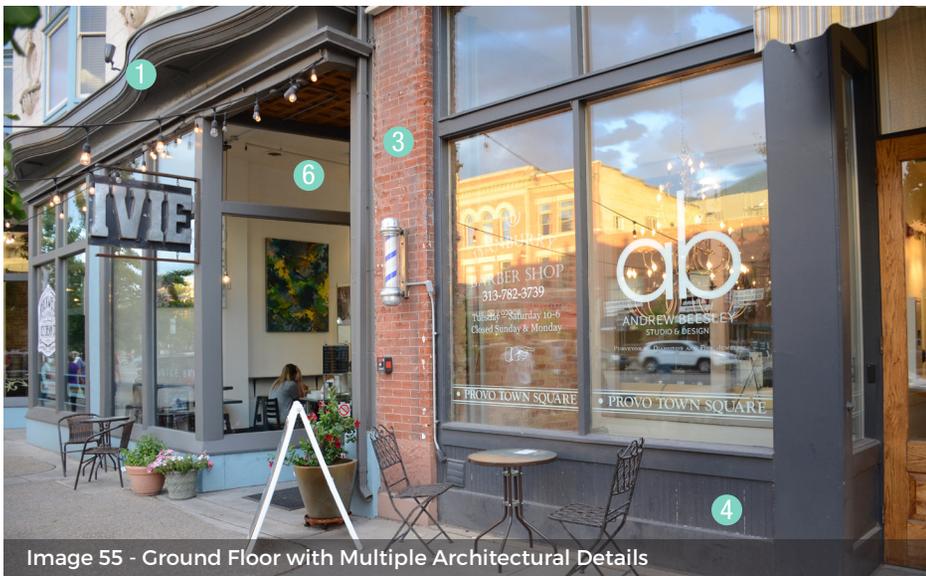


Image 53 - Buildings with Multiple Architectural Elements and Details

Image 54 - Buildings with Multiple Architectural Elements and Details

Image 55 - Ground Floor with Multiple Architectural Details

Image 56 - Detailed Building Facade

Image 57 - Parapet on Gallery Storefront

# Building Signage

Buildings signs are an essential element of Main Street, and each storefront should have signage to identify the businesses within. Signs are one of the best and most cost efficient ways to add interest and character to the streetscape and building facade.

- Signs should be an artful expression/representation of the business.
- The size of signs should be scaled appropriate to the size of the building.
- Plastic/internally illuminated signs are not appropriate.
- Buildings may have more than one sign type.

## Encouraged Sign Type

**Projecting Signs + Hanging Signs** - Each business along Main Street is highly encouraged to utilize a projecting sign or hanging sign. These are signs that are attached to the building face and project out over the sidewalk, or a sign that hangs from an overhead canopy. See images 58 - 59.

These sign types add significant interest to the pedestrian experience, and bring unique character to each establishment, and Main Street as a whole.

## Acceptable Sign Types

**Wall Signs** - Wall signs include signs that are attached to the face of a building wall. They should be mounted on the wall facing the primary street.

**Window Signs** - Window signs are painted or affixed on the interior of a window and intended to be viewed from the outside. Window signs should not block views into the store or business.

**Awning Signs** - Awning signs are mounted, printed on, painted on, or otherwise attached to an awning or canopy above a business door or window.

**Mural Sign** - Sign that is painted onto a wall that is visible from the public realm. Mural signs should not be painted on the primary building facade.



Image 58 - Projecting Sign



Image 59 - Hanging Sign



Image 60 - Window Sign



Image 61 - Awning Sign



Image 62 - Hanging Sign



Image 63 - Wall Sign



Image 64 - Mural Sign



Image 65 - Window Sign



Image 66 - Wall Sign



Image 67 - Awning Sign and Wall Sign



Image 68 - Wall Sign

## BEFORE

Existing Buildings on Main Street



Figure 17 - Existing Buildings on Main Street

## AFTER

This image shows a demonstration for how buildings facades could be redeveloped based on the criteria outlined on pages 34 - 43.

- 1 Parapet
- 2 Cornice
- 3 Brick
- 4 Painted Wood
- 5 Windows
- 6 Steel Awning
- 7 Canvas Awning
- 8 Awning Sign
- 9 Window Sign
- 10 Sandstone Bulkhead
- 11 Painted Wood Bulkhead



Figure 18 - Rendering Illustrating How Building Facades Could be Redeveloped

